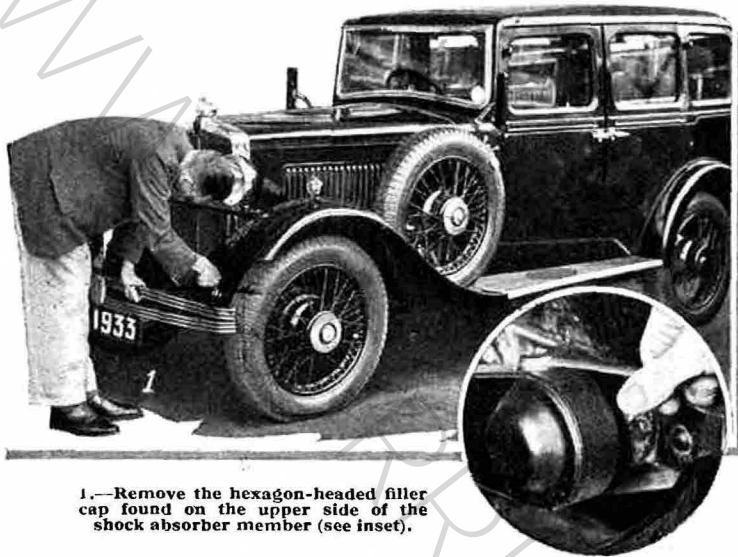


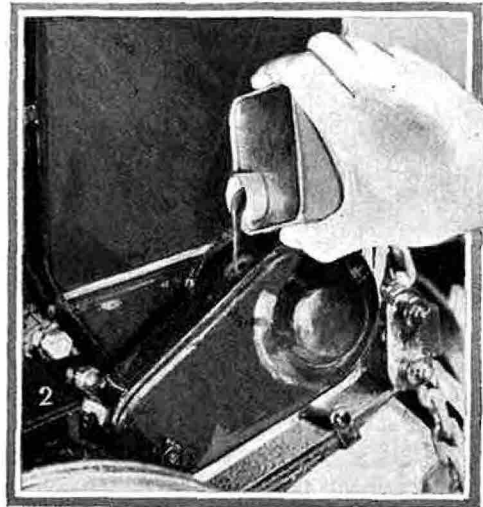
RUNNING HINTS IN PICTURES

No. 32

Attention to the Armstrong Hydraulic Shock Absorbers fitted to 1933 Cowley and Major models



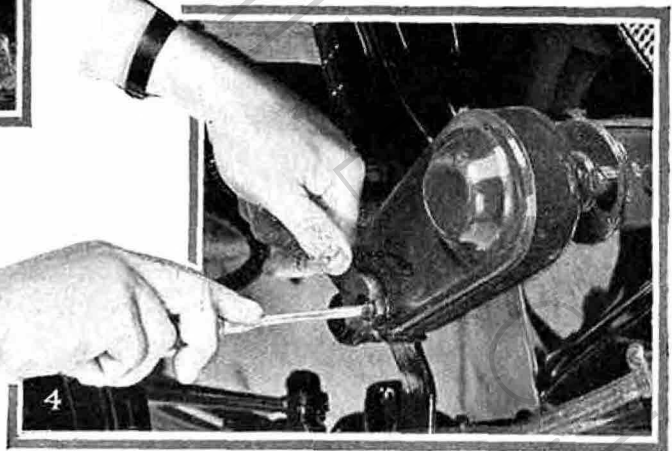
1.—Remove the hexagon-headed filler cap found on the upper side of the shock absorber member (see inset).



2.—If necessary, top up with Armstrong shock absorber oil to within half an inch below the filler plug. This level is correct and should not be varied.



3.—If adjustment is necessary (which is most unlikely), slacken the locking nut on the side of the casing, at the same time holding the adjusting screw with a screwdriver to ensure that it does not turn with the nut.



4.—Then give the adjusting screw a quarter of a turn in a clockwise direction—never more at a time; this will increase the tension.

NOTE.—Shock absorbers should be adjusted in pairs, the front two together, and then the rear if required. If there is any doubt as to what precise treatment is needed, it is recommended that they should be slackened slightly first, and the effect noted. Attention is only necessary at widely separated intervals.